

NOTES:

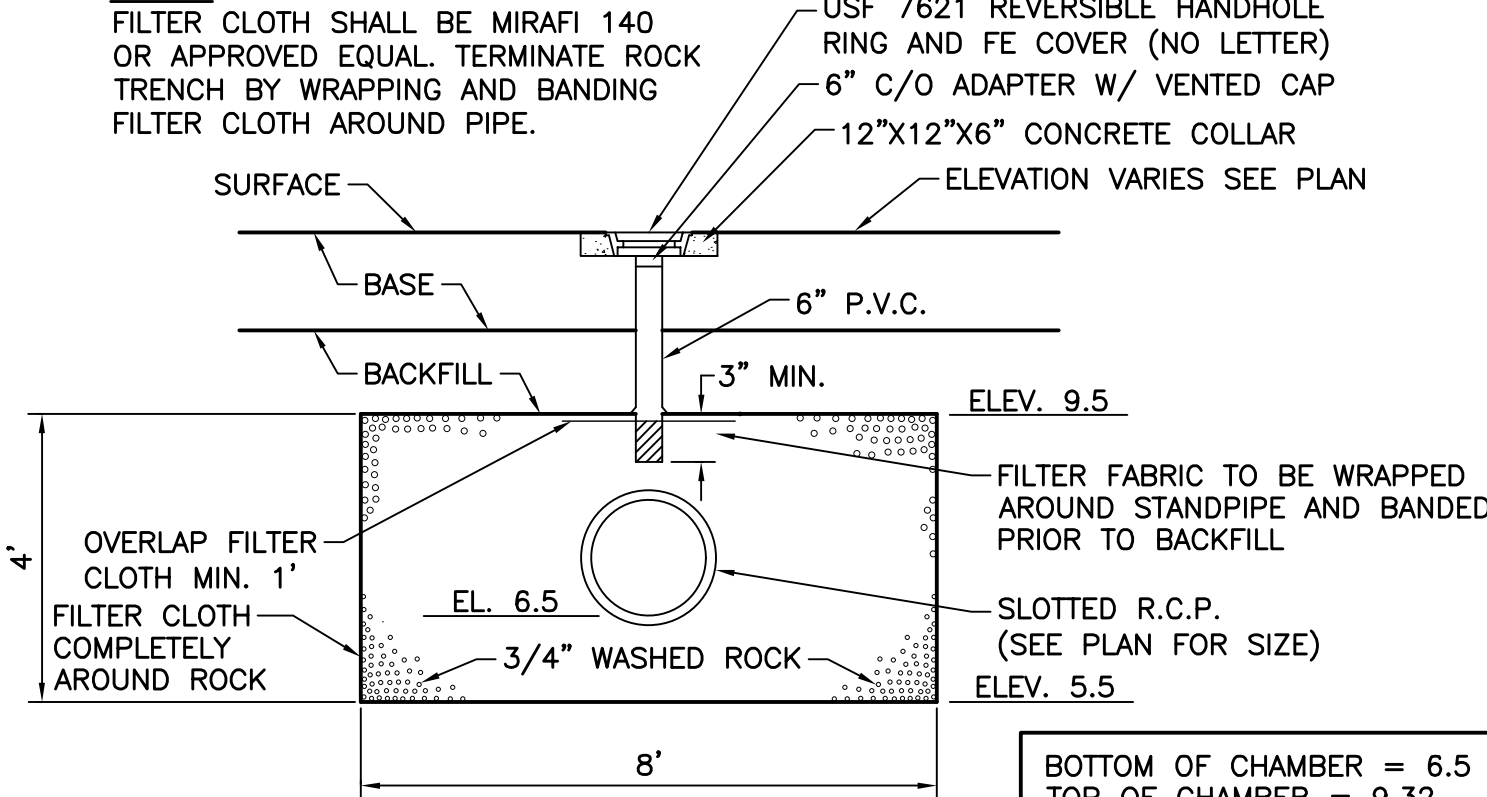
- BEDDING SHALL CONSIST OF IN-SITU GRANULAR MATERIAL OR WASHED AND GRADED LIMEROCK 3/8" - 7/8" SIZING WITH EQUAL OR GREATER STRUCTURAL ADEQUACY AS EXISTING. UNSUITABLE IN-SITU MATERIALS SUCH AS MUCK, DEBRIS AND LARGER ROCKS SHALL BE REMOVED.
- THE PIPE SHALL BE FULLY SUPPORTED FOR ITS ENTIRE LENGTH WITH APPROPRIATE COMPACTION UNDER THE PIPE HAUNCHES.
- THE PIPE SHALL BE PLACED IN A DRY TRENCH.
- BACKFILL SHALL BE FREE OF UNSUITABLE MATERIAL SUCH AS LARGE ROCK, MUCK AND DEBRIS.
- DENSITY TESTS ARE TAKEN IN 1 FOOT LIFTS ABOVE THE PIPE AT INTERVALS OF 400' MAXIMUM, (MINIMUM 1 SET) OR AS DIRECTED BY THE CONSTRUCTION COORDINATION DIVISION. RESULTS SHALL BE SUBMITTED TO CONSTRUCTION COORDINATION AS PART OF THEIR FIELD REVIEW.
- THE PERMITTEE/DEVELOPER/CONTRACTOR SHALL BE RESPONSIBLE TO COMPLY WITH ALL TRENCH SAFETY LAWS AND REGULATIONS.
- SEE SEPARATE DETAIL FOR PAVED AREAS (OPEN CUT FOR THOROUGHFARE AND NON-THOROUGHFARE ROADS).
- THE AFFECTED AREA SHALL BE RESTORED TO EQUAL OR BETTER CONDITION OR AS SPECIFIED IN PERMIT/CONTRACT DOCUMENTS.
- APPROVED MAGNETIC TAPE IS REQUIRED FOR ALL MAIN PRESSURE PIPES AND CONDUIT IN THE COUNTY'S RIGHT-OF-WAY. INSTALL TAPE 18" BELOW FINISHED GRADE.
- ROOT BARRIER IS REQUIRED FOR APPROVED PIPE INSTALLATION GREATER THAN 10 FEET FROM AN EXISTING TREE. SEE SEPARATE DETAIL FOR ROOT BARRIER.
- CONTINUOUS 4" WIDE PAINT STRIPING IS REQUIRED FOR DIP/PCOP WATER MAINS (BLUE), DIP SANITARY FORCE MAINS (GREEN), DIP RECLAIMED WATER MAINS (PURPLE), GAS MAINS (YELLOW), OR AS REQUIRED BY THE APWA.
- PERMANENT ABOVE GROUND UTILITY MARKER SHALL BE INSTALLED IF REQUIRED.

**TYPICAL TRENCH DETAIL
(NON-PAVED AREAS)**

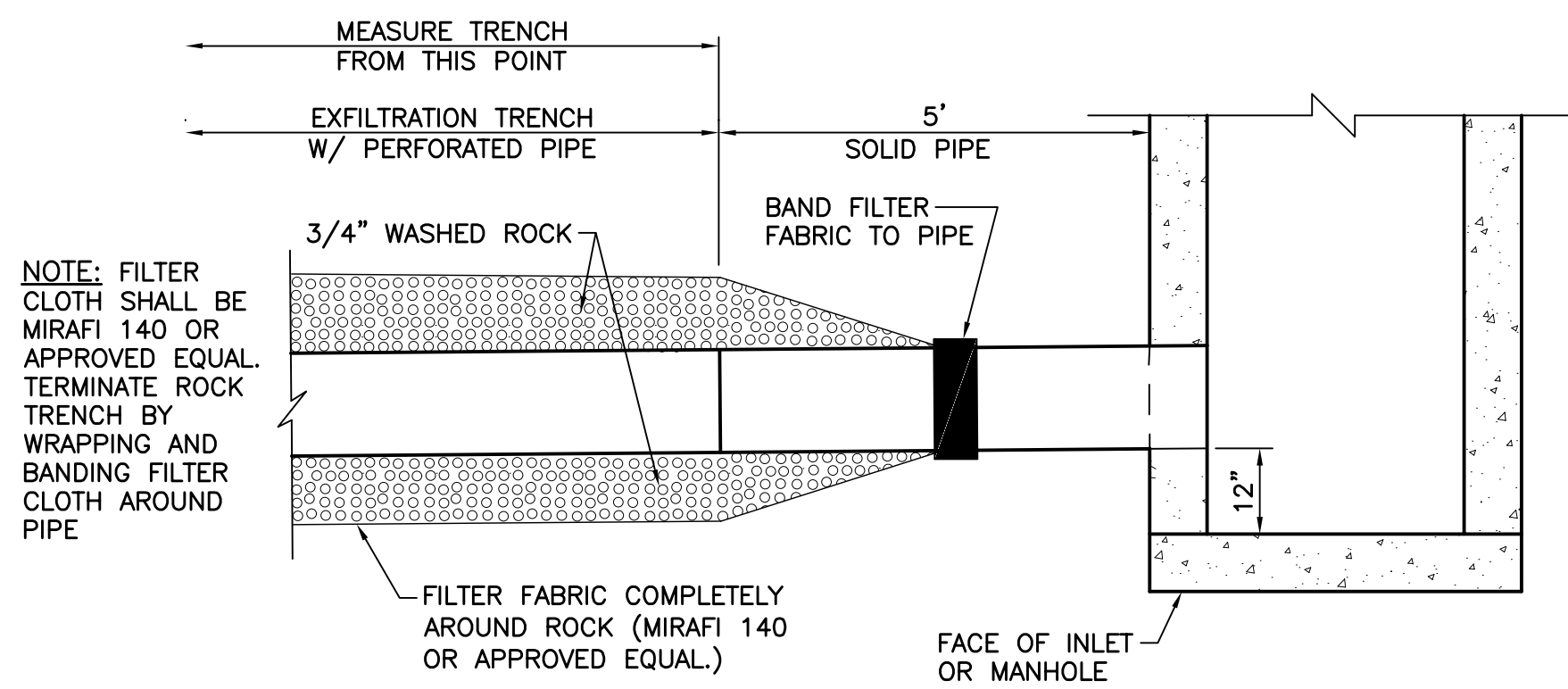
NOT TO SCALE

NOTE:

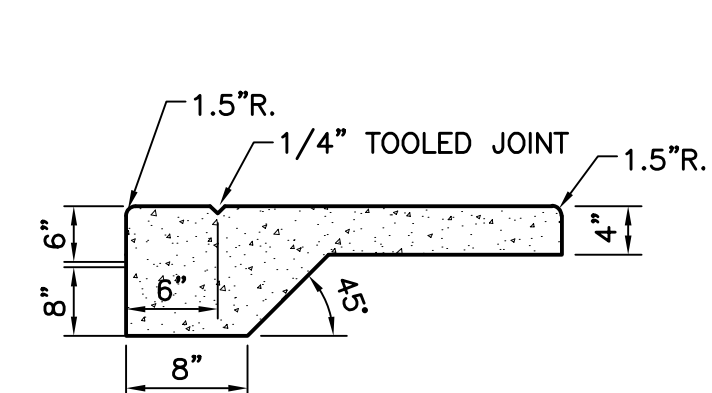
FILTER CLOTH SHALL BE MIRAFI 140 OR APPROVED EQUAL. TERMINATE ROCK TRENCH BY WRAPPING AND BANDING FILTER CLOTH AROUND PIPE.

**EXFILTRATION TRENCH**

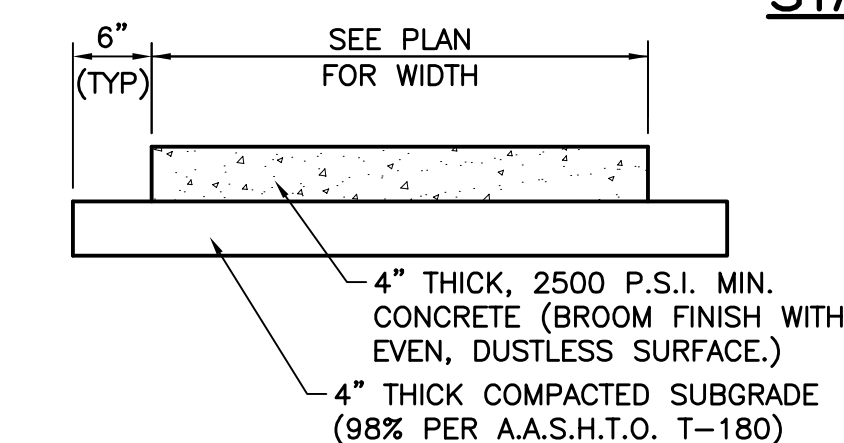
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**FILTER FABRIC END CLOSURE DETAIL**

NOT TO SCALE

**RAISED SIDEWALK DETAIL**

NOT TO SCALE

**TYPICAL SIDEWALK DETAIL**

NOT TO SCALE

CONSTRUCTION PROCEDURES

THE BACKFILL FOR THE FIRST AND SECOND STAGES SHALL BE PLACED IN 6" LAYERS (COMPACTED THICKNESS) AND SHALL BE COMPACTED TO 100% OF MAXIMUM DENSITY AS DETERMINED BY AASHTO T-99.

STAGE 1:

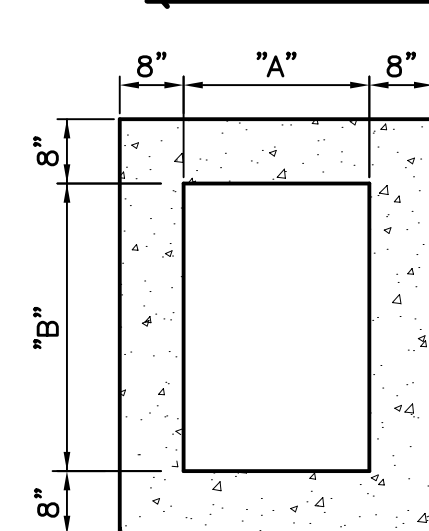
THE CONTRACTOR SHALL PROVIDE ADEQUATE COMPACTED FILL BENEATH THE HAUNCHES OF THE PIPE, USING MECHANICAL TAMPS SUITABLE FOR THIS PURPOSE. THIS COMPACTION APPLIES TO THE MATERIAL PLACED BENEATH THE HAUNCHES OF THE PIPE AND ABOVE ANY BEDDING REQUIRED.

STAGE 2:

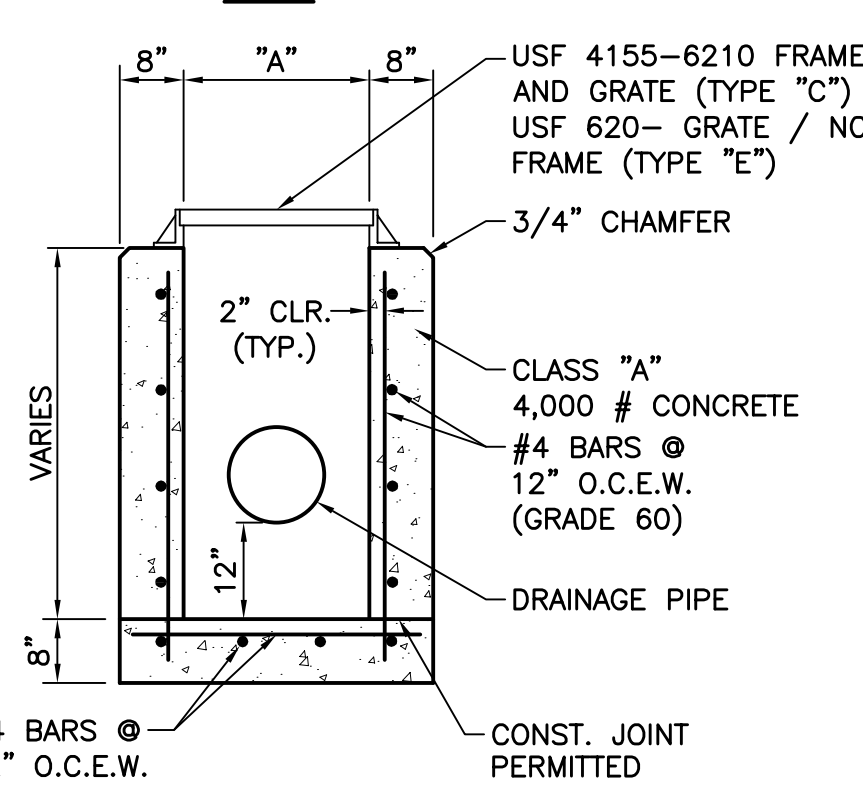
THE CONTRACTOR SHALL OBTAIN A WELL-COMPACTED BED AND FILL ALONG THE SIDES OF THE PIPE AND TO A POINT INDICATING THE TOP OF SUB-GRADE MATERIAL.

NOTES

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- REPLACED BASE MATERIAL OVER DITCH SHALL BE 16" LIMEROCK (LBR100) MINIMUM FOR THOROUGHFARE PLAN ROADS. ANY ALTERNATE BASE MATERIAL REQUESTED BY THE ENGINEER OF RECORD REQUIRES APPROVAL BY THE ROADWAY PRODUCTION DIVISION.
- ASPHALT CONCRETE PAVEMENT JOINTS SHALL BE MECHANICALLY SAWED AND BUTT-JOINTED.
- BASE MATERIAL (PER ROADWAY PRODUCTION DESIGN STANDARDS) SHALL BE PLACED IN TWO OR THREE LAYERS (6" MAX. PER LAYER) AND EACH LAYER THOROUGHLY ROLLED OR TAMPED TO THE SPECIFIED DENSITY. (MINIMUM 98% AASHTO T-180).
- 1" FRICTION COURSE FC-9.5 OVER 1-1/2" TYPE SP STRUCTURAL COURSE (TRAFFIC LEVEL C) WITH TACK COAT AT 0.05 GAL/SY AND RC-70 PRIME COAT AT 0.10 GAL/SY FOR LIMEROCK BASE. FOLLOW THE LATEST FDOT SPECIFICATIONS FOR APPLICATION RATES OF PRIME AND TACK COATS. CONTRACTOR TO SUBMIT MATERIALS AND RATES TO ENGINEER FOR APPROVAL PRIOR TO BEGINNING WORK.
- PIPE SHALL BE PLACED IN A DRY TRENCH.
- ALL ROADWAY REPAIR WORK SHALL BE PERFORMED IN CONFORMANCE WITH APPLICABLE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND COUNTY PPM# EL-0-3605.
- DENSITY TESTS SHALL BE TAKEN IN 1 FT LIFTS ABOVE THE PIPE AT INTERVALS OF 400 FT MAXIMUM (1 SET MINIMUM) OR AS DIRECTED BY THE CONSTRUCTION COORDINATION DIVISION. RESULTS SHALL BE SUBMITTED TO CONSTRUCTION COORDINATION DIVISION AS PART OF THEIR FIELD REVIEW.
- ENGINEER-OF-RECORD SHALL PROVIDE FULL-TIME INSPECTION DURING THE ENTIRETY OF THE OPEN-CUT OPERATION, BEGINNING WITH THE EXCAVATION AND CONTINUING THROUGH THE COMPLETION OF THE PAVING.
- IF THE PAVEMENT IS NOT COMPLETELY RESTORED IMMEDIATELY FOLLOWING THE OPEN CUT, A SMOOTH TEMPORARY PATCH (MINIMUM 1.5" SP 12.5 STRUCTURAL COURSE ASPHALT) SHALL BE INSTALLED, PROPERLY MATCHING THE EXISTING GRADING OF THE ROADWAY. THE TEMPORARY PATCH SHALL BE ALLOWED TO REMAIN IN PLACE AND BE MAINTAINED FOR A PERIOD NO LONGER THAN 45 DAYS. THE COUNTY RETAINS THE RIGHT TO USE POSTED SURETY TO COMPLETE ANY RESTORATION WORK THAT HAS NOT BEEN COMPLETED IN THE 45 DAY PERIOD. ALTERNATIVE TEMPORARY TRENCH PROTECTION (STEEL PLATES OR OTHERS) MAY BE APPROVED BY THE CONSTRUCTION COORDINATION DIVISION.
- FOR FINAL RESTORATION (INCLUDES THE PATCHED/SURFACE REPLACEMENT AREA OVER THE TRENCH). THE ROAD SHALL BE MILLED/RESURFACED PER NOTE 5 ABOVE FOR A FULL LANE WIDTH OF THE TRAVEL LANES ENCRoACHED BY THE TRENCH AREA, INCLUDING A TRANSITION AREA OF 50 FT ON EACH SIDE MEASURED FROM TOP OF TRENCH.
- APPROVED MAGNETIC TAPE IS REQUIRED FOR ALL MAIN PRESSURE PIPES AND CONDUIT IN THE COUNTY'S RIGHT-OF-WAY. INSTALL TAPE 24" BELOW FINISHED GRADE.
- CONTINUOUS 4" WIDE PAINT STRIPING IS REQUIRED FOR DIP/PCOP WATER MAINS (BLUE), SANITARY MAINS (GREEN), DIP RECLAIMED WATER MAINS (PURPLE), GAS MAINS (YELLOW), OR AS REQUIRED BY THE APWA.

**OPEN CUT DETAIL FOR
THOROUGHFARE ROADS
(PAVED AREAS)**

PLAN

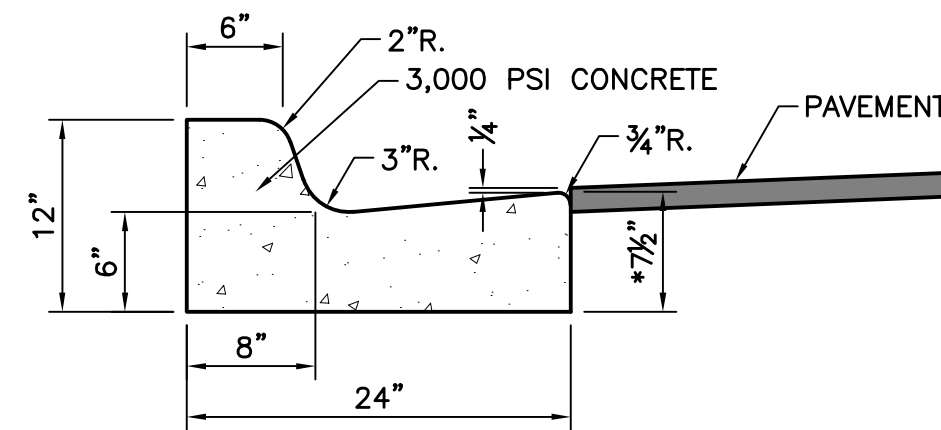


SECTION

TYPE	DIMENSIONS
INLET	"A" "B"
C	2'-0" 3'-1"
E	3'-0" 4'-6"

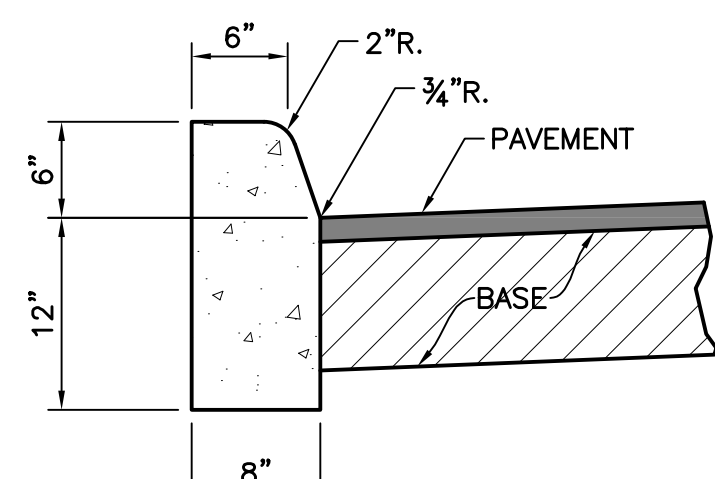
STANDARD TYPE "C" & "E" INLET

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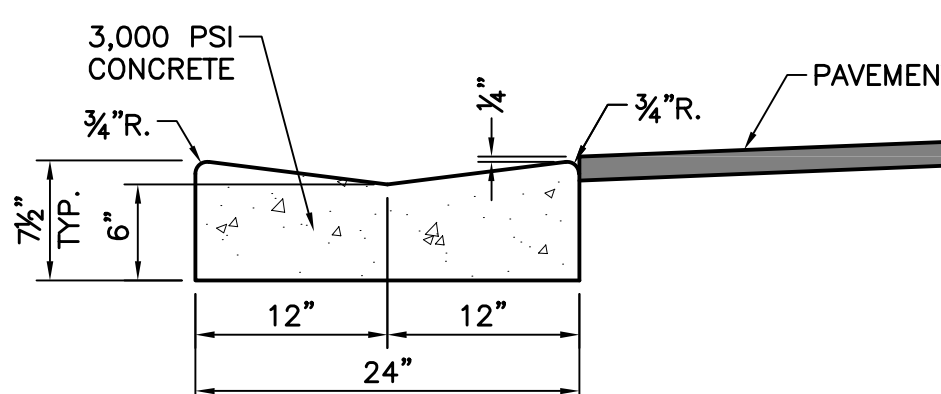
**TYPE "F" CURB**

NOT TO SCALE

***NOTE:** WHEN USED ON HIGH SIDE OF ROADWAY, THE CROSS-SLOPE OF THE GUTTER SHALL MATCH THE CROSS-SLOPE OF THE ADJACENT PAVEMENT AND THE THICKNESS OF THE LIP SHALL BE 6", UNLESS OTHERWISE SHOWN ON PLANS

**TYPE "D" CURB**

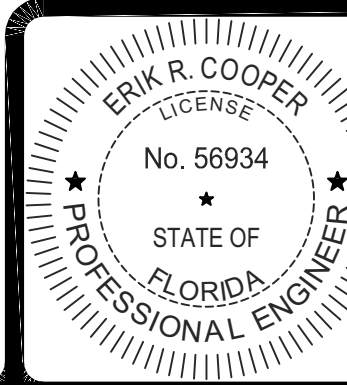
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**2' VALLEY GUTTER-SYMMETRICAL**

NOT TO SCALE

CURB AND GUTTER NOTES:

- CONCRETE SHALL BE MINIMUM OF 3,000 PSI
- TYPE "A" CONTRACTION JOINTS SHALL BE @ 10' O.C.
- TYPE "B" EXPANSION JOINTS SHALL BE @ P.C./P.T. AND 100' O.C.

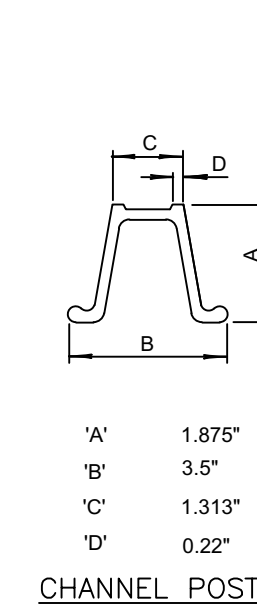


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ERIK R. COOPER, P.E. ON
4/12/2024.

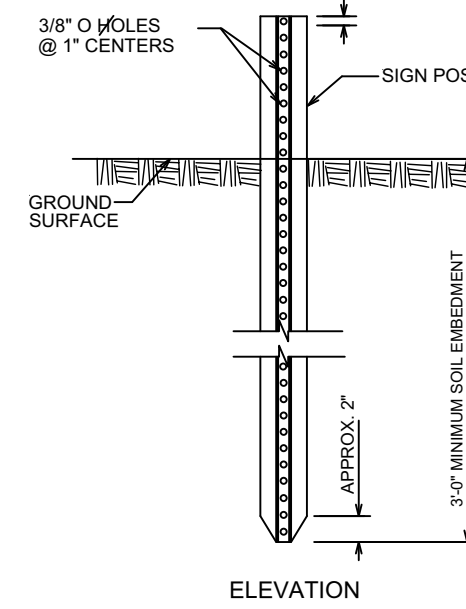
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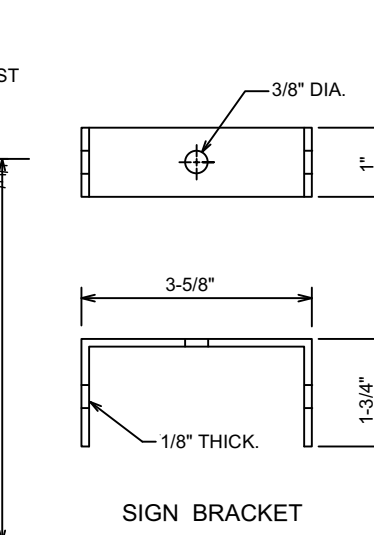
ISOMETRIC VIEW



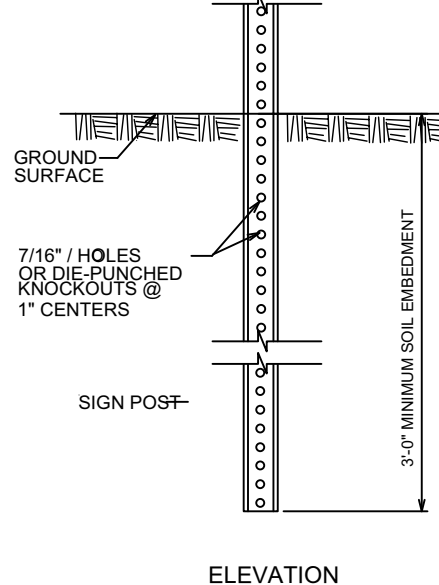
CHANNEL POST



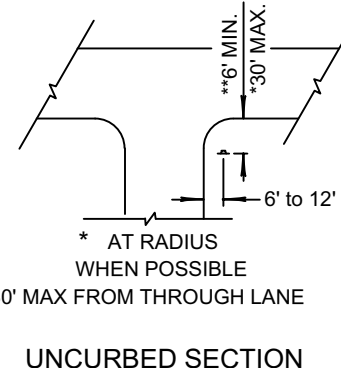
STEEL FLANGED CHANNEL POST DETAILS



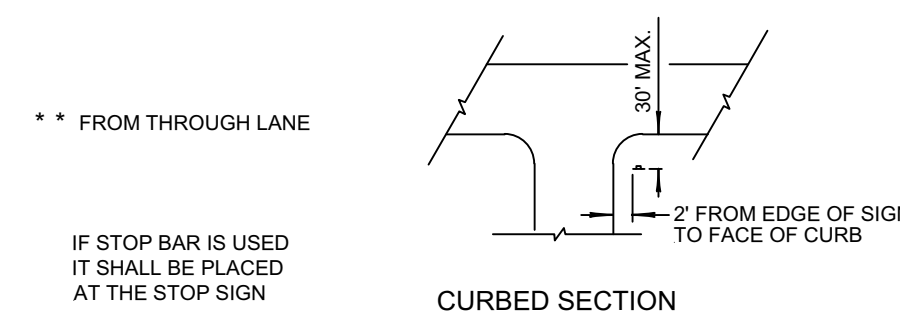
SIGN BRACKET



STEEL SQUARE TUBE POST DETAILS

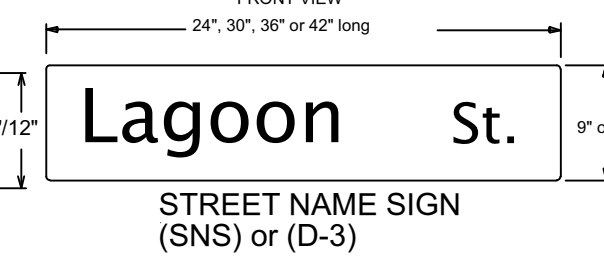


UNCURBED SECTION



CURBED SECTION

TYPICAL STOP SIGN PLACEMENT

STREET NAME SIGN
(SNS) or (D-3)**STREET NAME SIGN WITH STOP SIGN**

NOT TO SCALE

